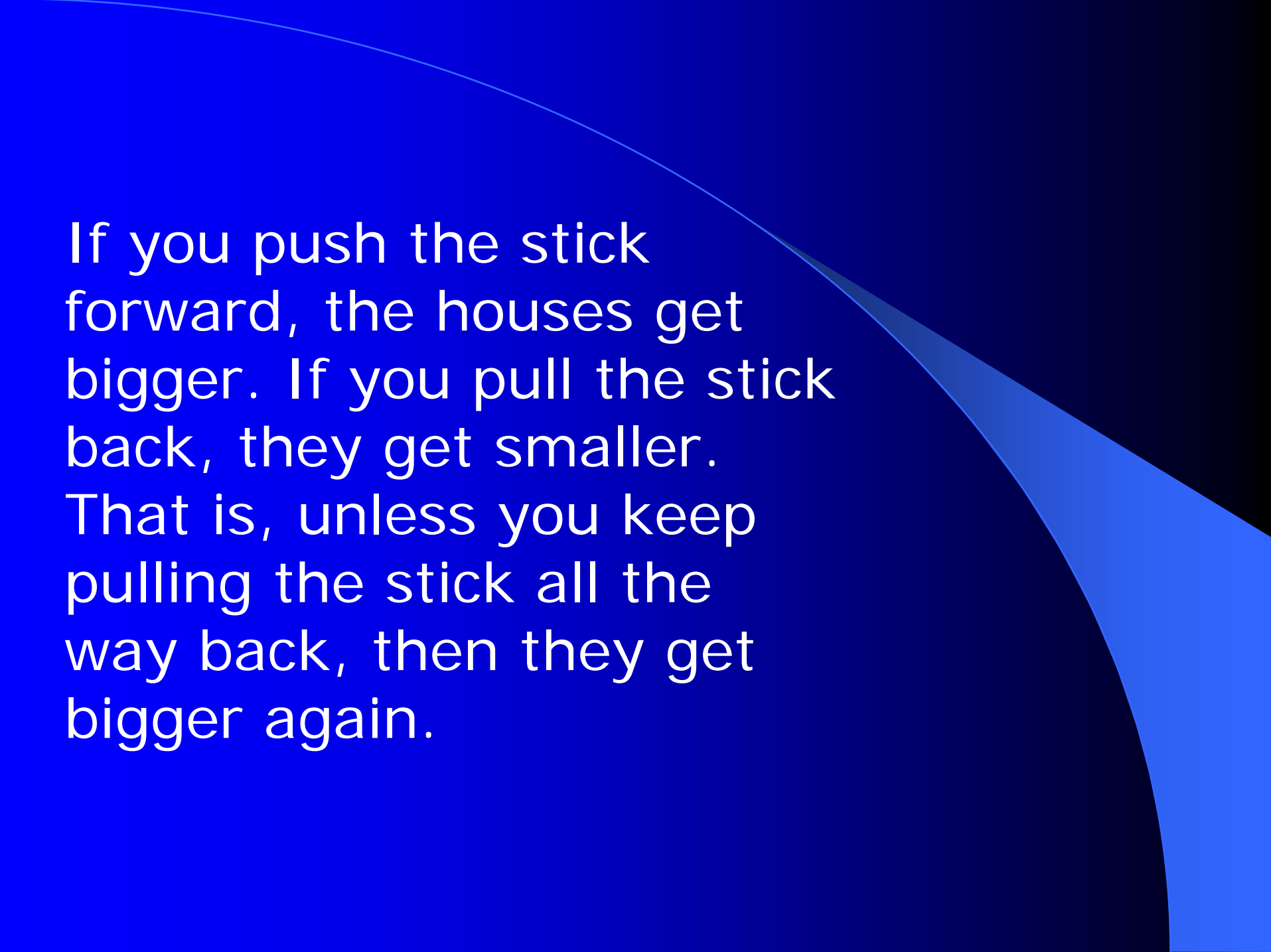


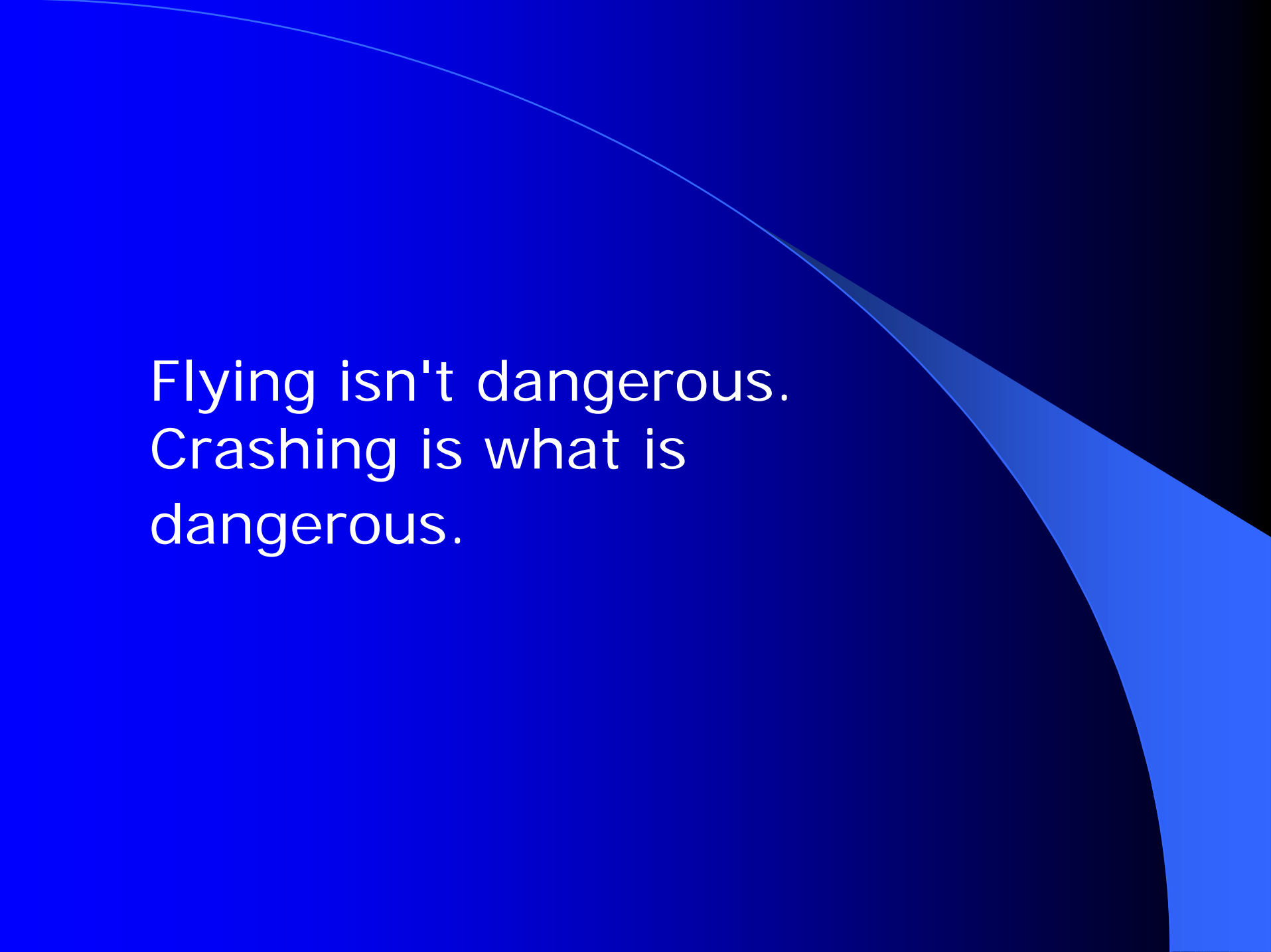
Pilot's Wisdom

Bits and Bites to Live By

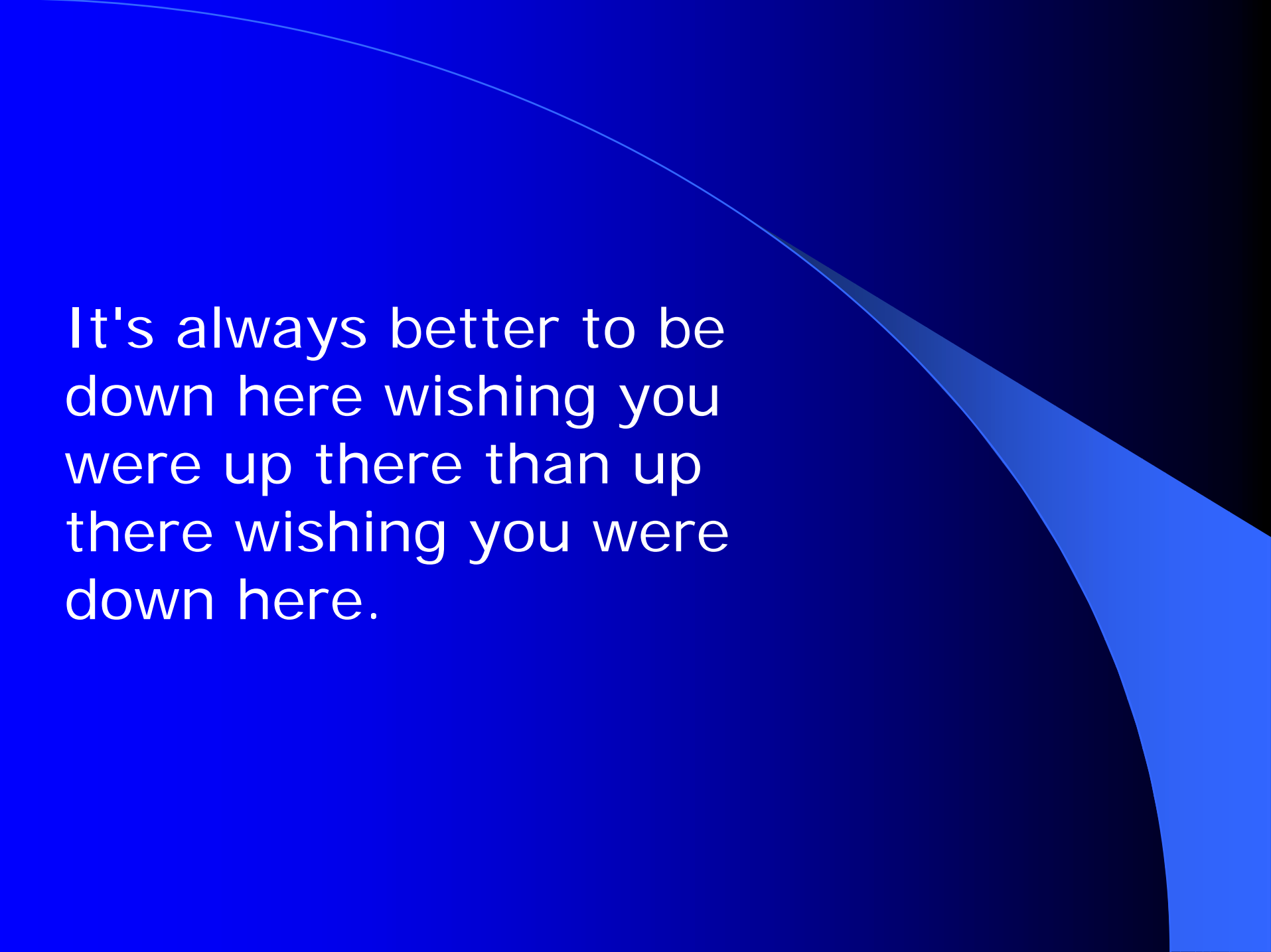
~Author Unknown




If you push the stick forward, the houses get bigger. If you pull the stick back, they get smaller. That is, unless you keep pulling the stick all the way back, then they get bigger again.



Flying isn't dangerous.
Crashing is what is
dangerous.

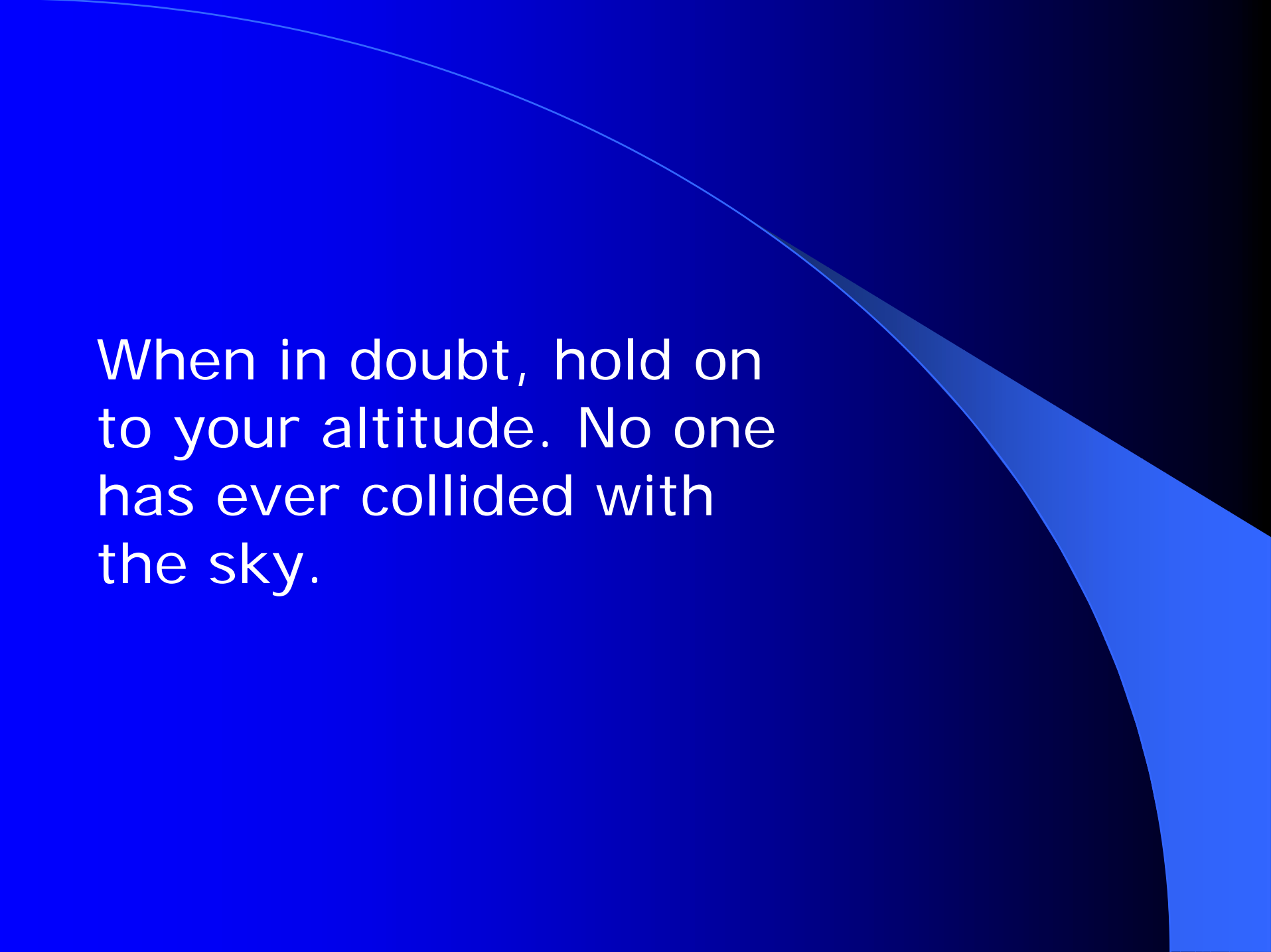


It's always better to be
down here wishing you
were up there than up
there wishing you were
down here.

The background is a dark blue gradient. A thin, light blue curved line starts from the top left and arcs towards the right. A larger, light blue triangular shape is positioned on the right side, pointing towards the center.

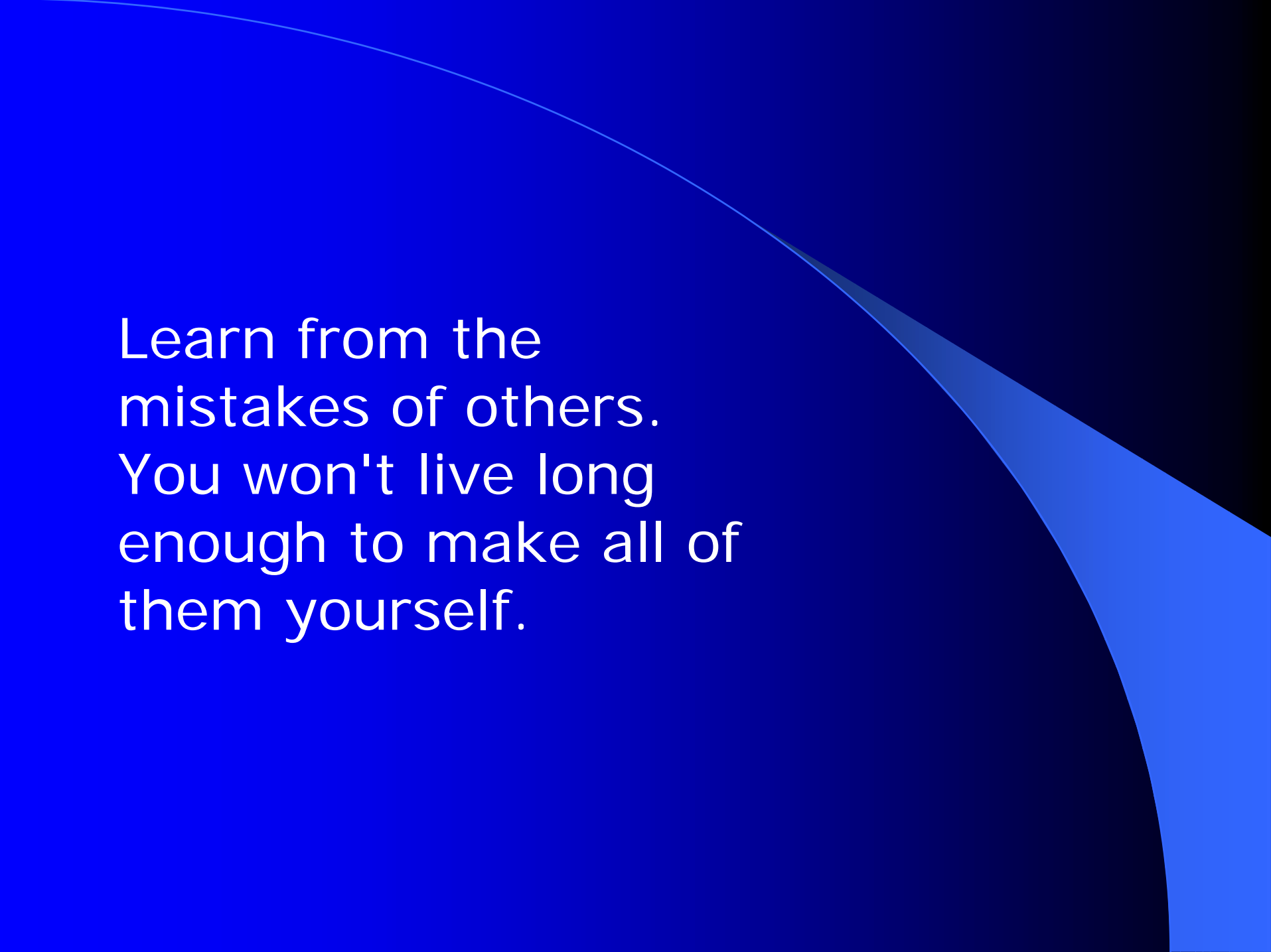
The **ONLY** time you
have too much fuel is
when you're on fire.

The propeller is just a big fan in front of the plane used to keep the pilot cool. Because when it stops, you can actually watch the pilot start sweating.



When in doubt, hold on
to your altitude. No one
has ever collided with
the sky.

A 'good' landing is one from which you can walk away. A 'great' landing is one after which they can use the plane again.



Learn from the
mistakes of others.
You won't live long
enough to make all of
them yourself.

You know you've
landed with the
wheels up if it takes
full power to taxi to
the ramp.

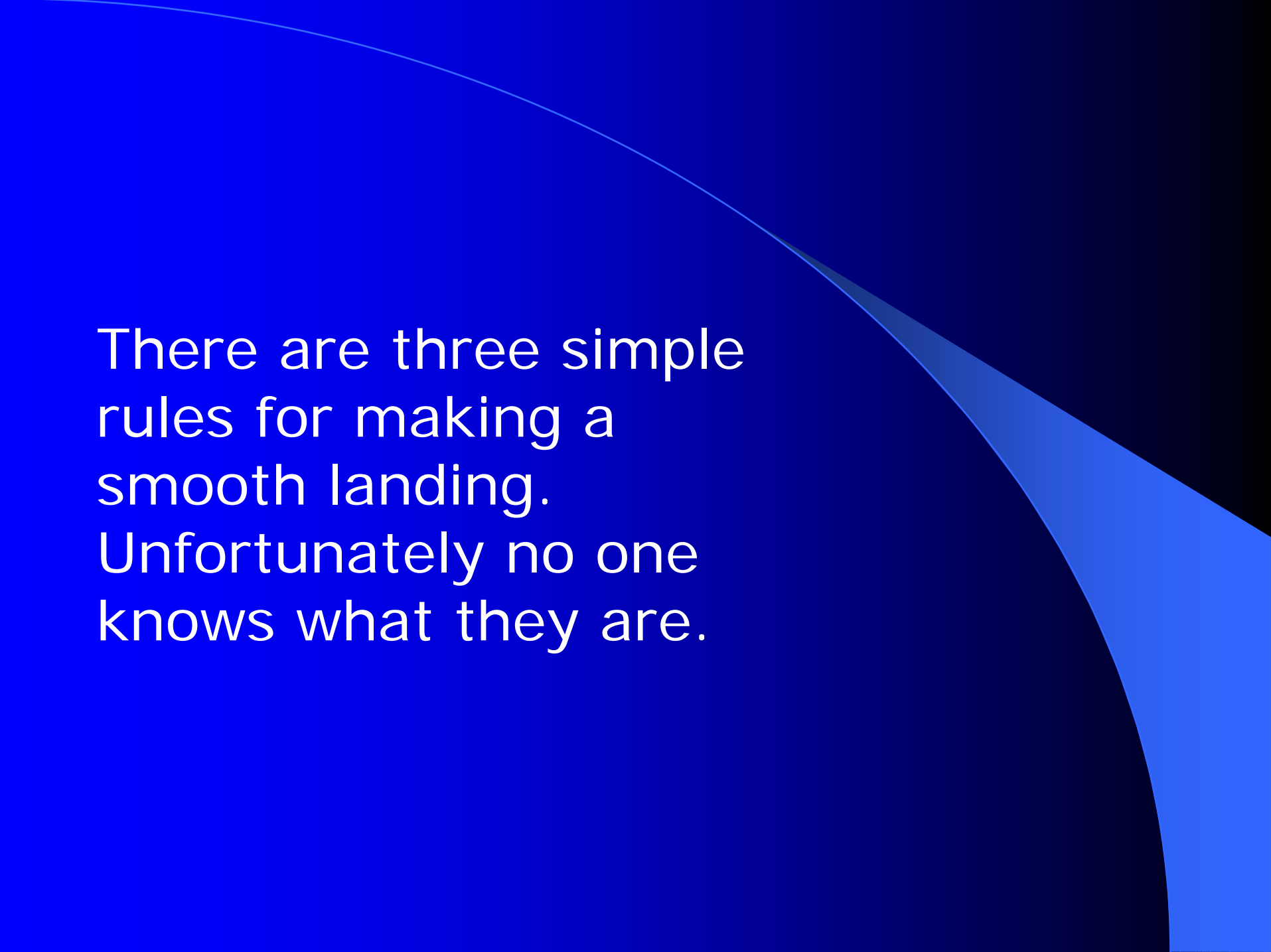
You know you have just done a very bad touch and go when a voice from the control tower comes on the radio and tells you how many bounces you just did. That means you have their attention and they are counting.

The probability of survival is inversely proportional to the angle of arrival. Large angle of arrival, small probability of survival and vice-versa.

Never let an aircraft
take you somewhere
your brain didn't get to
five minutes earlier.

Stay out of clouds. The silver lining everyone keeps talking about might be another airplane going in the opposite direction. Reliable sources also report that mountains have been known to hide out in clouds.

Always try to keep the number of landings you make equal to the number of take offs you've made.




There are three simple
rules for making a
smooth landing.
Unfortunately no one
knows what they are.

You start with a bag full of luck and an empty bag of experience. The trick is to fill the bag of experience before you empty the bag of luck.

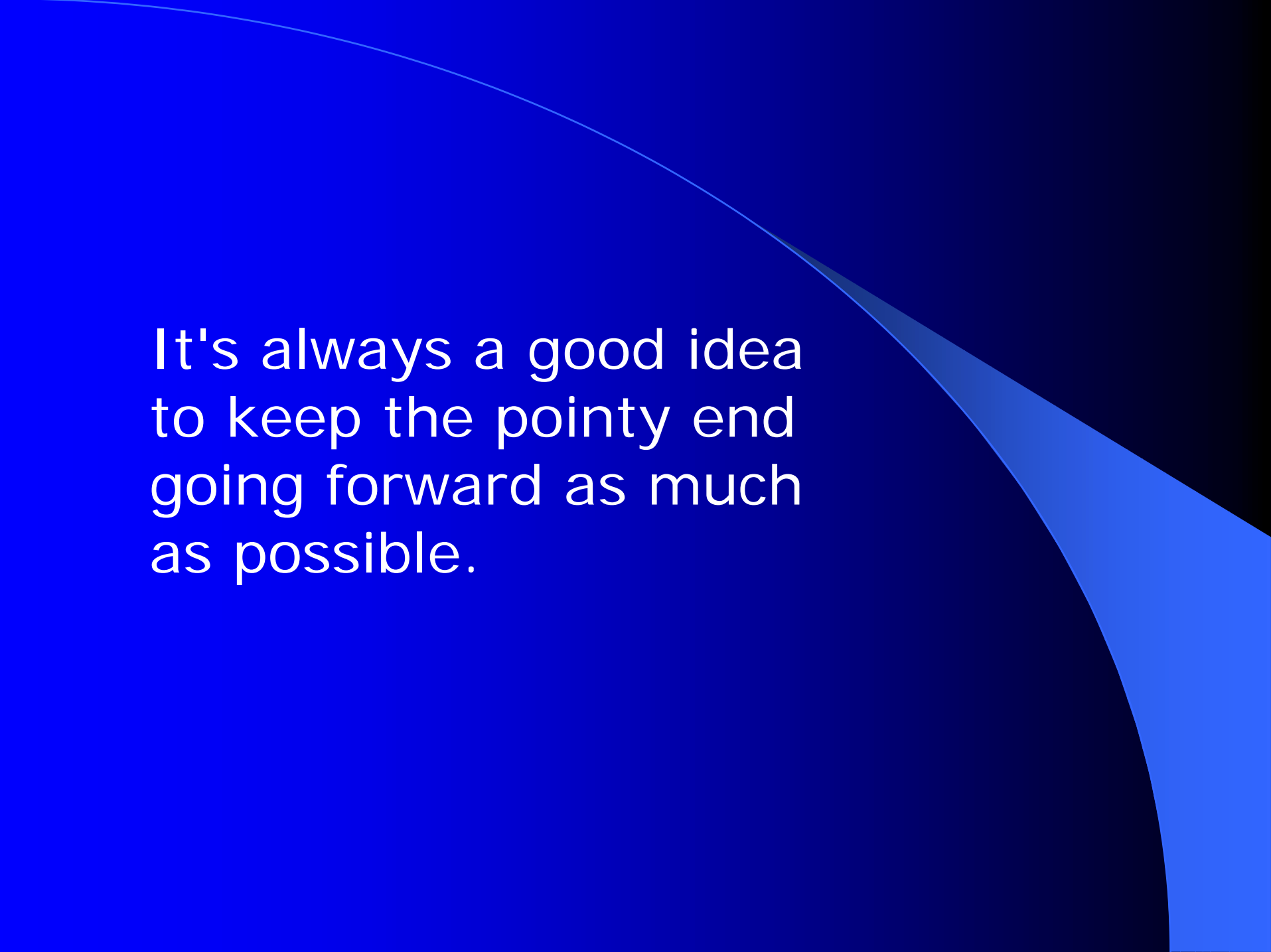
Helicopters can't fly;
they're just so ugly the
earth repels them.

If all you can see out of the window is ground that's going round and round and all you can hear is commotion coming from the passenger compartment, things are not at all as they should be.

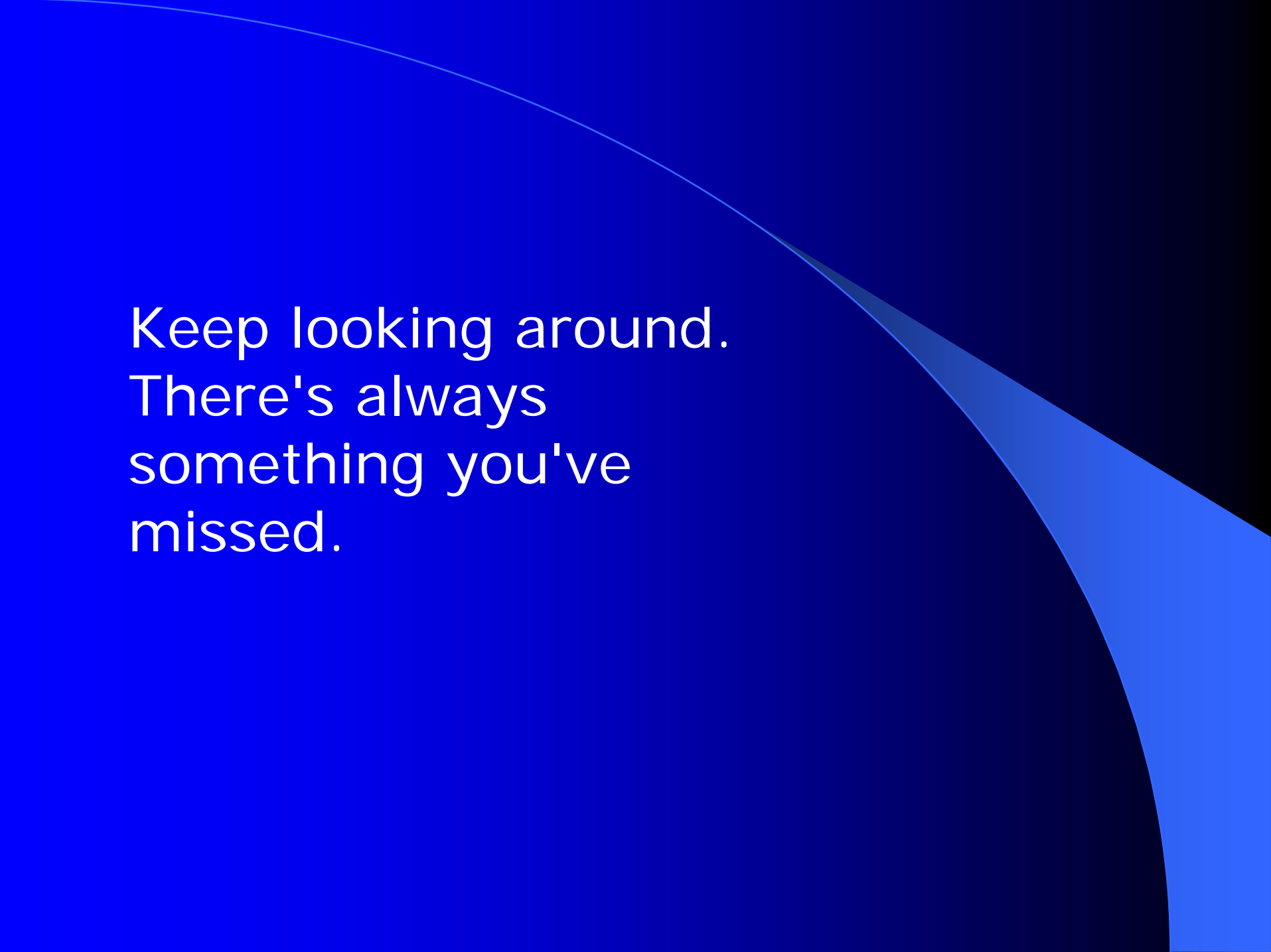
In the ongoing battle between objects made of aluminum going hundreds of miles per hour and the ground going zero miles per hour, the ground has yet to lose.



Good judgment comes
from experience.
Unfortunately, experience
usually comes from bad
judgment.



It's always a good idea
to keep the pointy end
going forward as much
as possible.



Keep looking around.
There's always
something you've
missed.

Remember, gravity is not just a good idea. It's the law. And it is not subject to repeal.

The four most useless things to a pilot are:

- the altitude above you,
- runway behind you,
- gas back at the airport,
- and a tenth of a second ago.

There are old pilots and
there are bold pilots. There
are, however, no old, bold
pilots.

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